

Park to Playa Trail

June 17, 2014



Park to Playa Trail



Representatives:

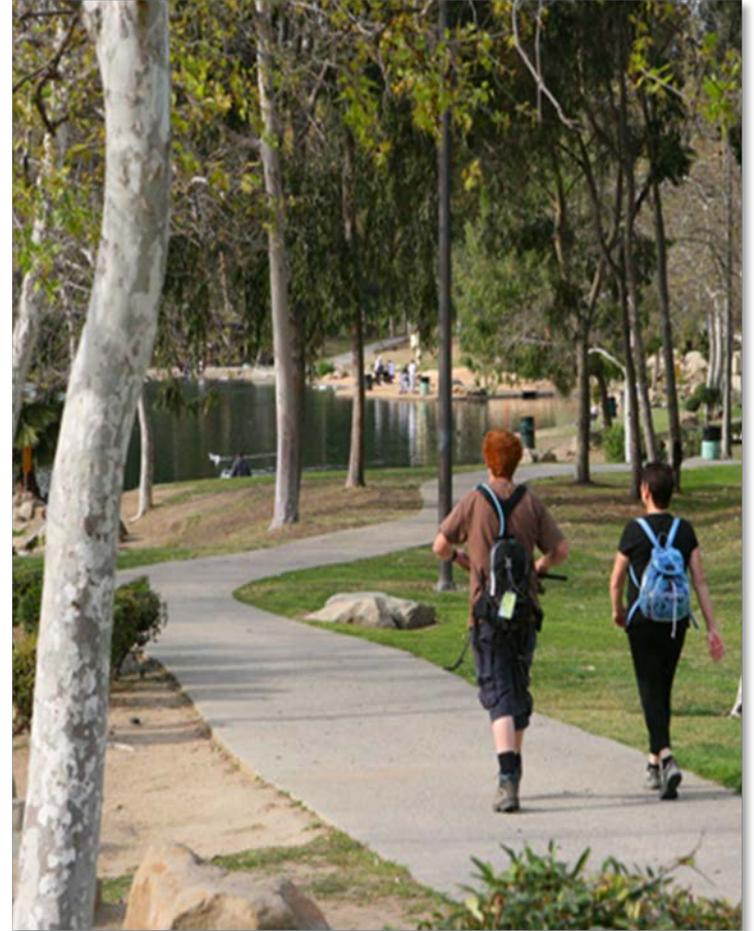
- **Karly Katona**, Deputy for Sustainability & Economic Development, Office of Supervisor Mark Ridley-Thomas
- **David McNeill**, Executive Officer, Baldwin Hills Conservancy
- **Hayden Sohm**, Deputy Director, County of Los Angeles Department of Parks and Recreation
- **Rudolph Galindo**, Management Analyst, City of Culver City
- **Ana Straabe**, Deputy Chief/Project Manager, Mountains Recreation & Conservation Authority
- **Randy Anderson**, Principal-in-Charge, Alta Planning + Design
- **Greg Maher**, Senior Designer/Project Manager, Alta Planning + Design

Project Partners

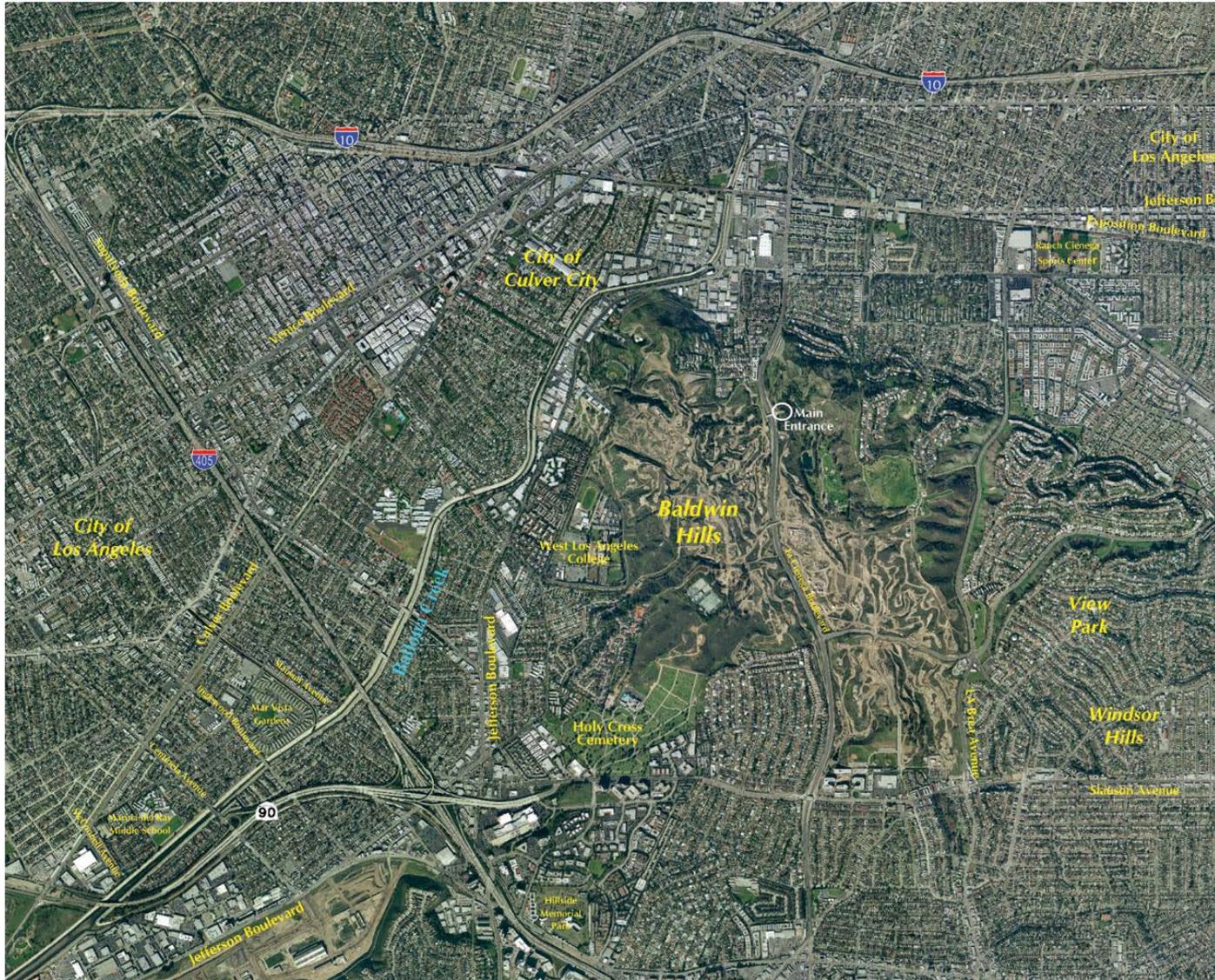
- Mountains Recreation & Conservation Authority
- Baldwin Hills Regional Conservation Authority
- Baldwin Hills Conservancy
- Office of Los Angeles County Supervisor Mark Ridley-Thomas
- Alta Planning + Design
- County of Los Angeles Department of Parks and Recreation
- State of California Department of Parks and Recreation
- City of Los Angeles
- City of Culver City

Meeting Agenda

- Introductions
- Project Overview
- Case Studies
- Key Concerns
- Next Steps
- Q and A



Baldwin Hills Conservancy Territory



BALDWIN HILLS
CONSERVANCY



Baldwin Hills Park Lands



Mia Lehrer + Associates Hood Design

LEGEND



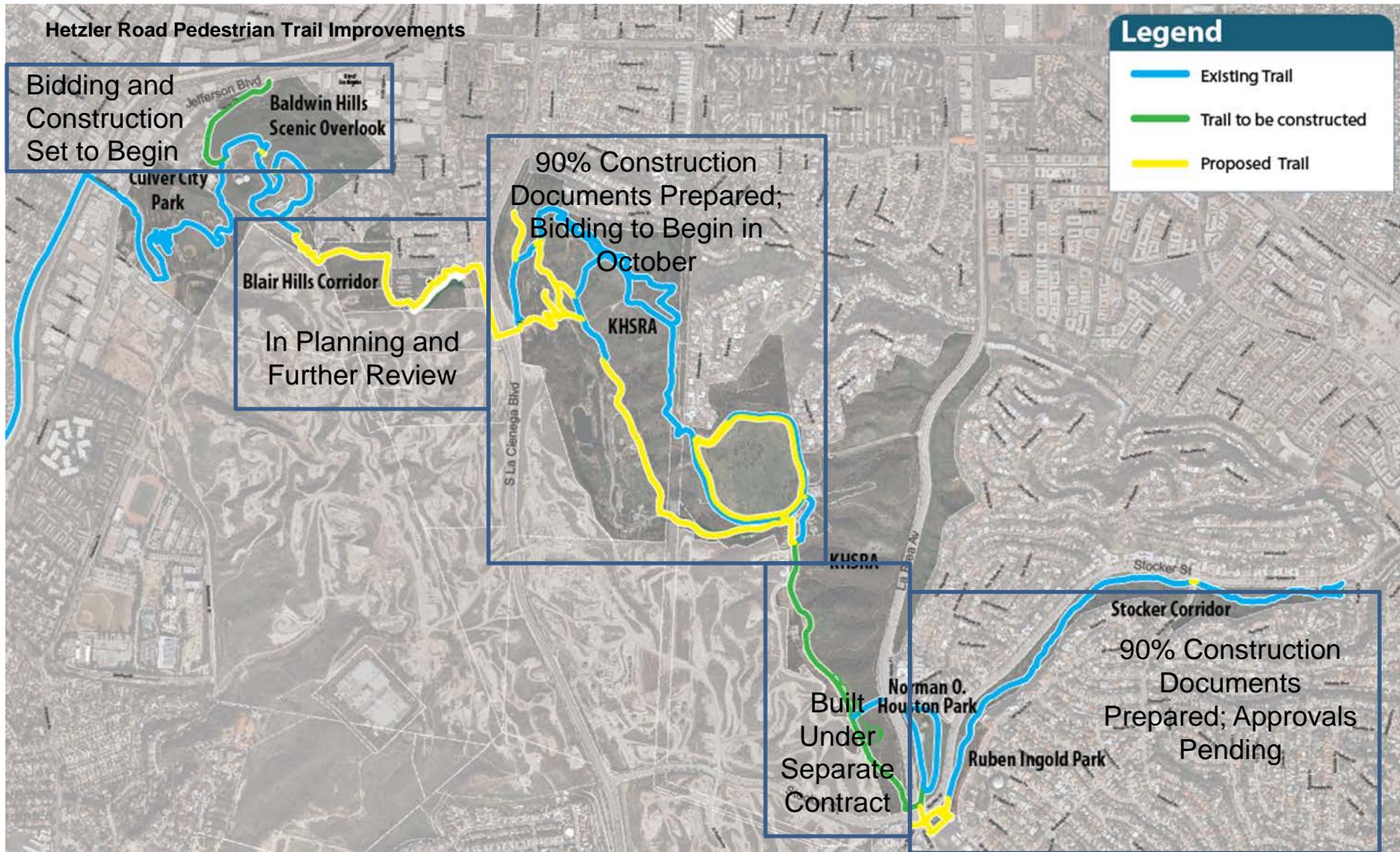
BALDWIN HILLS CONSERVANCY



Baldwin Hills Area Ownership



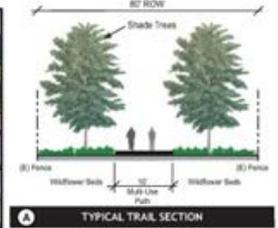
Project Implementation



La Cienega Gateway



Case Study: Live Oak, CA



1 EXISTING CORRIDOR



2 EXISTING CONNECTION OPPORTUNITY



TRAIL ENTRY FEATURE

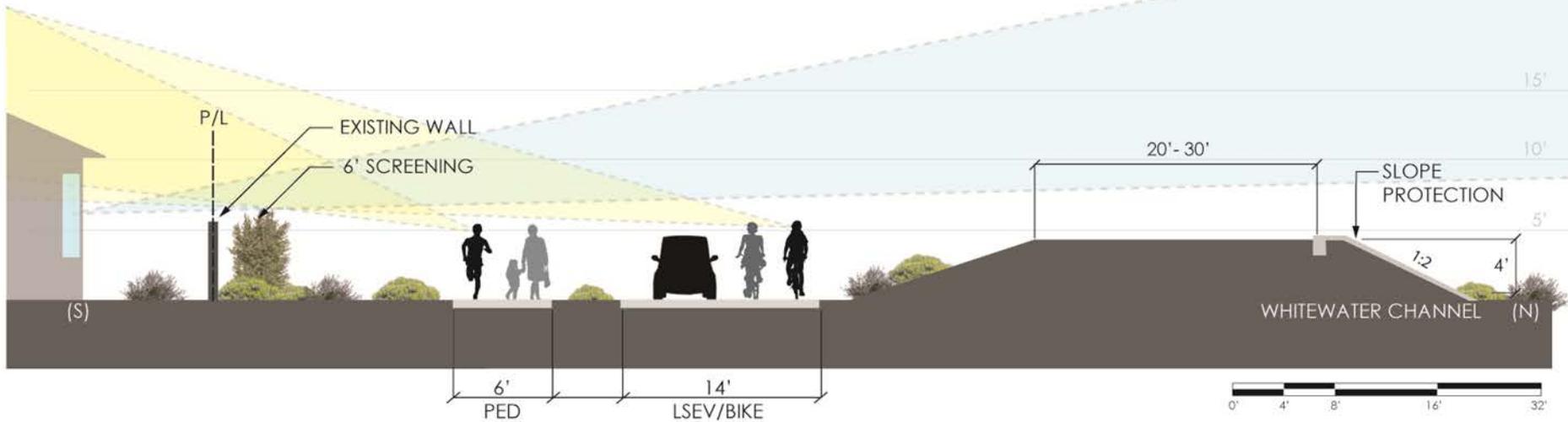
LIVE OAK RECREATIONAL TRAIL
SEGMENT 1
APRICOT STREET TO ELM STREET



Case Study: Palm Springs, CA



OFF LEVEL PATH WITH ENHANCED SCREENING



Successful Residential Adjacent Trails in LA County

- Bellflower Bike Trail, Bellflower, CA
- Ballona Creek Trail, Playa del Rey to Culver City, CA
- Whittier Greenway Trail, Whittier, CA
- Compton Creek Bike Path, Compton, CA
- Southern Avenue Greenway, Lynwood, CA
- Hermosa Valley Greenbelt, Hermosa and Manhattan Beach, CA
- Horse Lane Trail, La Canada-Flintridge, CA
- Avocado Heights Trail, La Puente, CA
- Flint Canyon Trail, La Canada-Flintridge, CA



WHITTIER GREENWAY TRAIL



HERMOSA VALLEY GREENBELT



AVOCADO HEIGHTS TRAIL

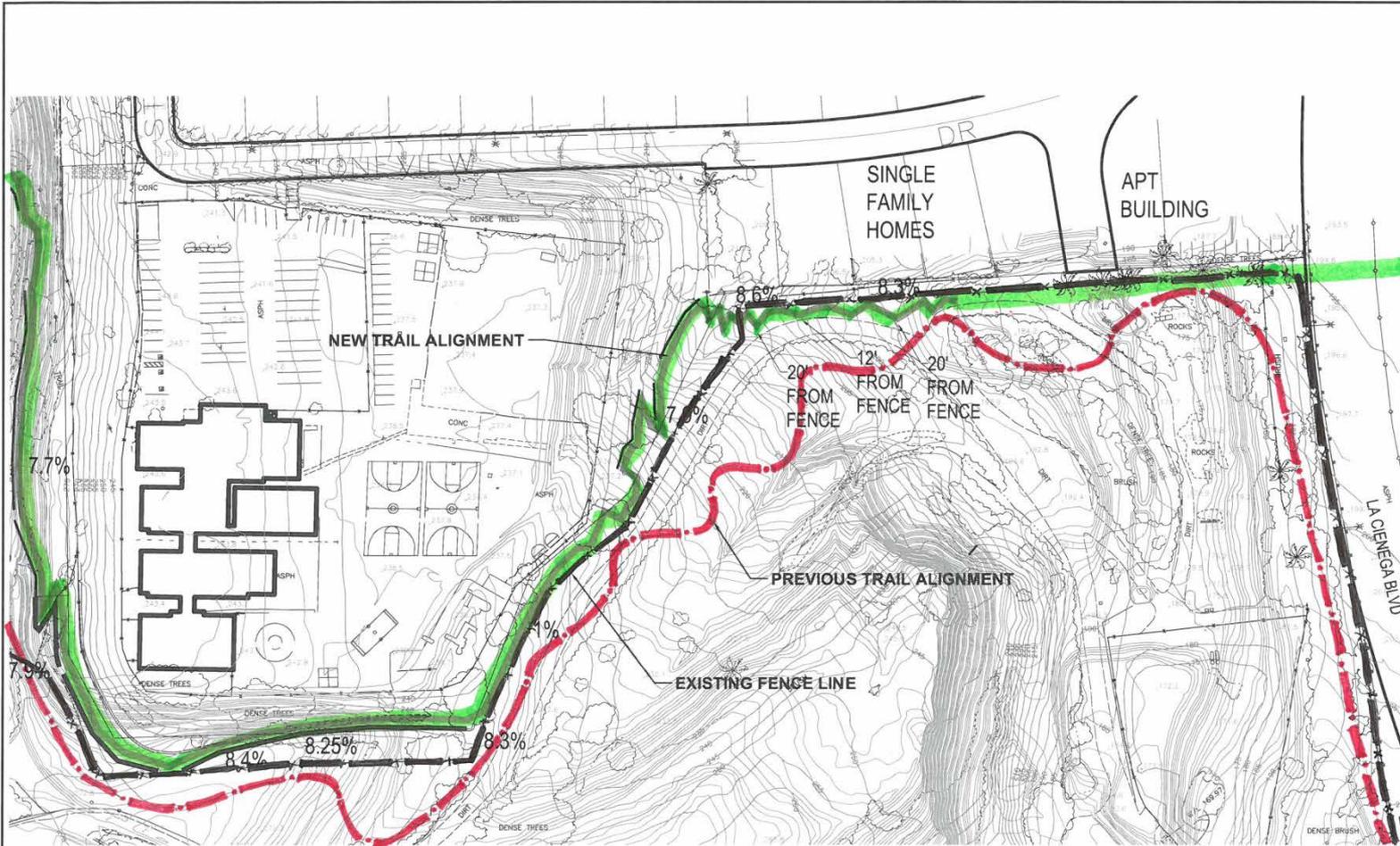


FLINT CANYON TRAIL

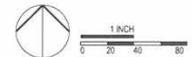
Trail Alignment- Segment C



Previous / New Trail Alignment



- NEW TRAIL ALIGNMENT
- PREVIOUS TRAIL ALIGNMENT
- EXISTING OIL FIELD FENCE LINE



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BLAIR HILLS
CORRIDOR

Park to Play
Trail

Los Angeles, CA

PHASE		30% CD
REVISIONS		
NO.	DATE	ITEM

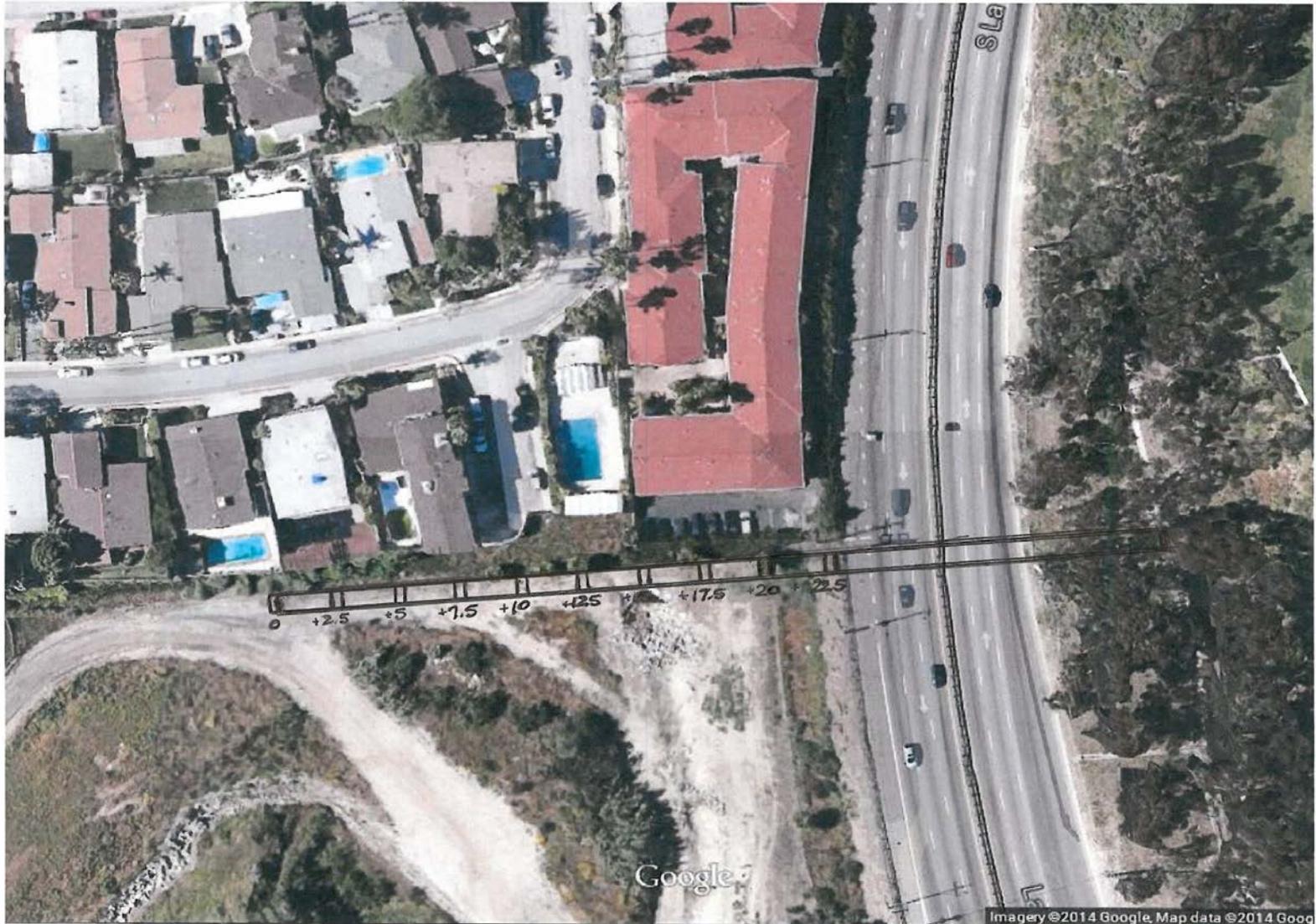


REVIEWED	XX
DRAWN	EDOC
11-21-13	9-18-2012
PROJECT NO.	DATE

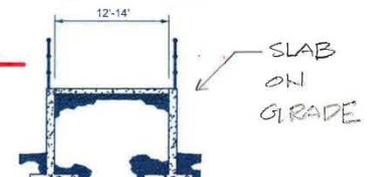
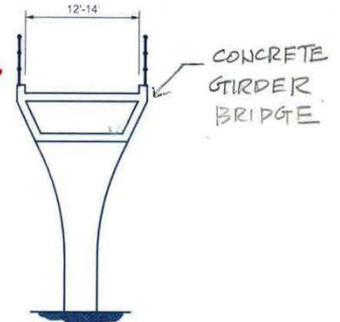
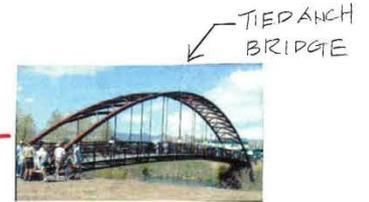
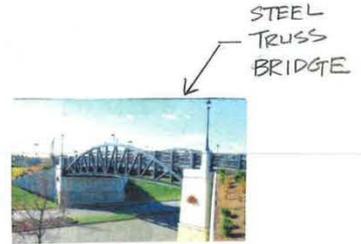
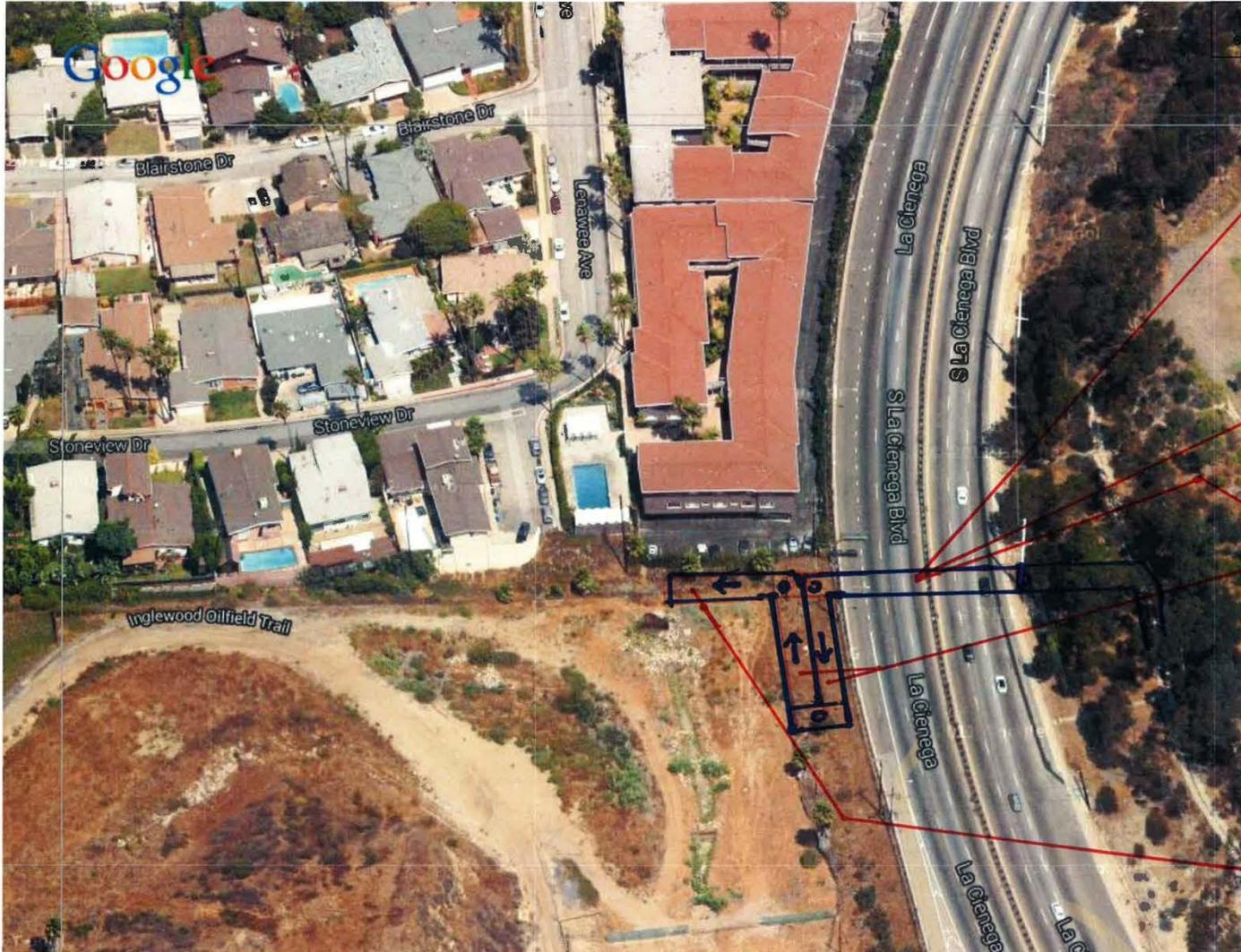
PROPOSED
TRAIL
ALIGNMENT
(MORE
ACCESSIBLE
ROUTE)

SHEET NO.

Pedestrian Bridge Option 1



Pedestrian Bridge Option 2



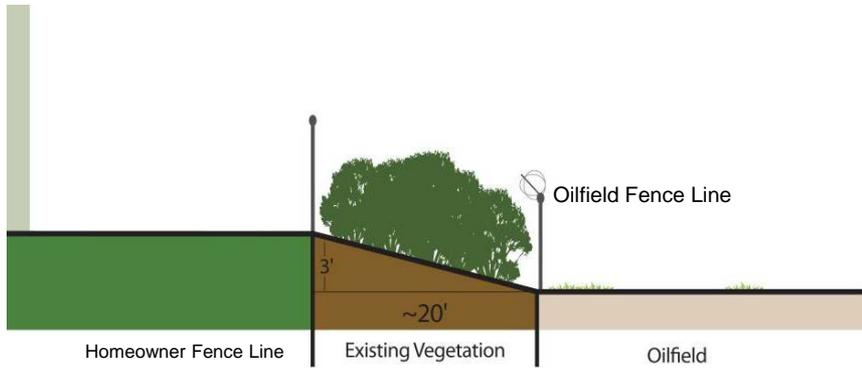
Key Concerns

- Visibility
- Noise
- Security
- Property Values

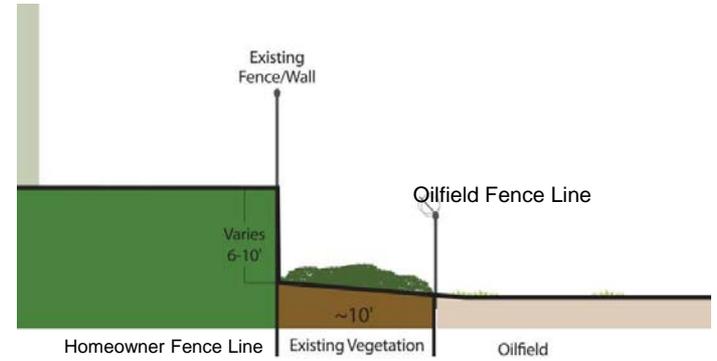


Concern: Visibility

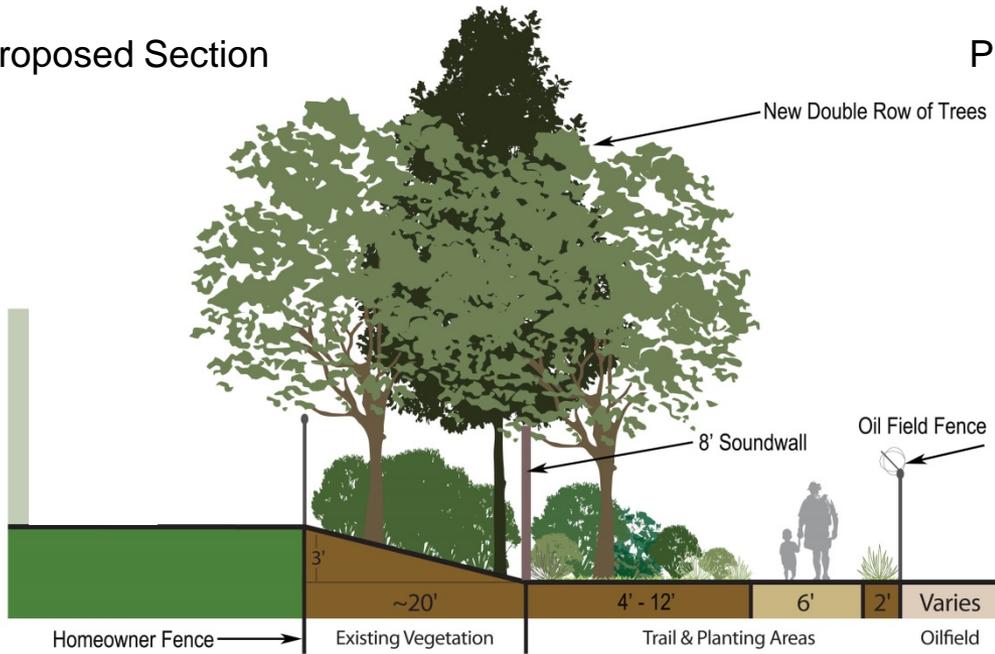
Current Section



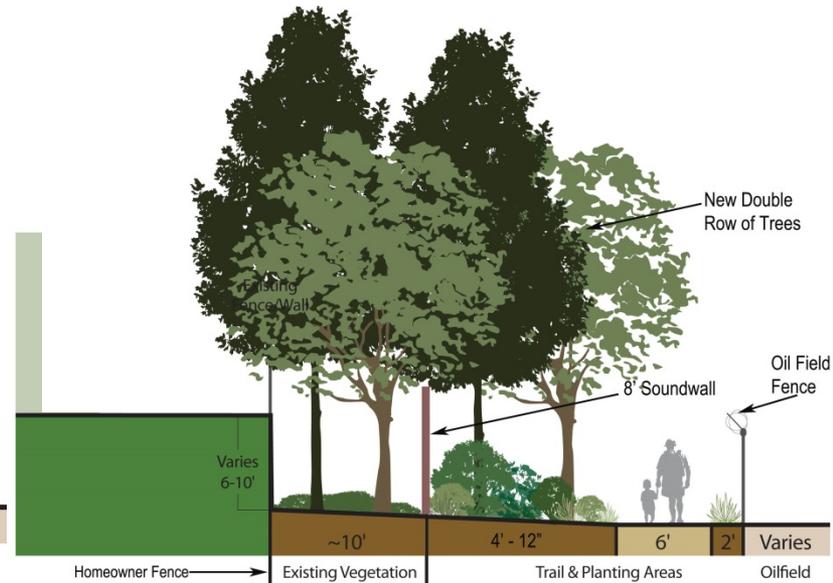
Current Section



Proposed Section



Proposed Section



Concern: Visibility



Concern: Visibility



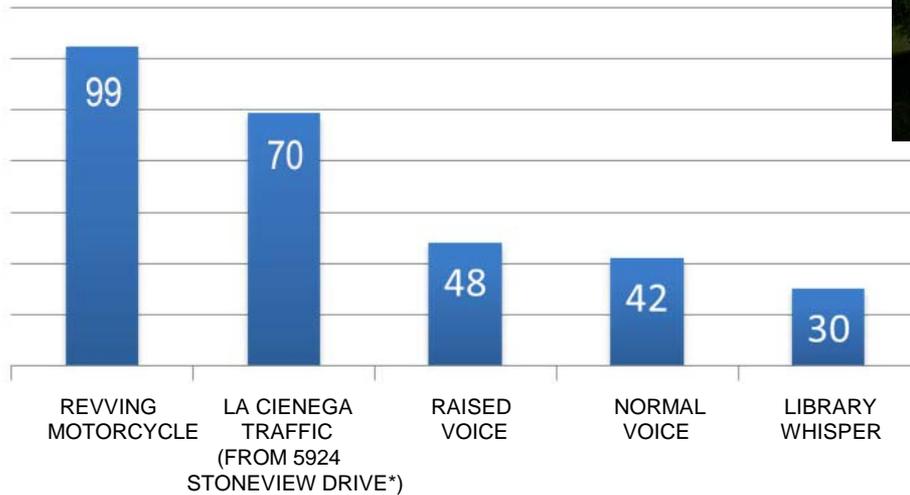
Concern: Noise

Mitigation Techniques:

- Trail Courtesy Signage
- Additional Customized Landscaping
- Sound Wall
- Visual and Sound Screening on the Pedestrian Bridge/Ramp



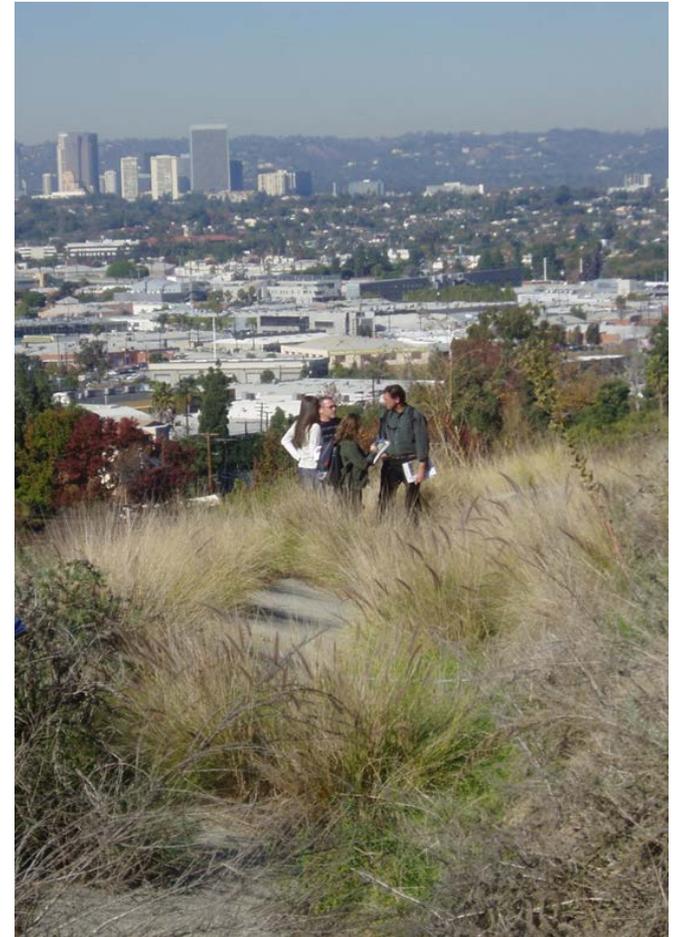
Decibel Levels at 20 feet away



*Source: CEQA Document for Park to Playa Trail, January 2013

Concern: Security

- Monitoring by the Sheriff's Department
- Trail closure from dusk to dawn
- Security gates at entries to the trail
- No amenities provided for this portion
- "Eyes on the Trail" / "See and be Seen"
- Clear separation between public/private uses
- Scheduled maintenance including trash and landscape/brush trimming



Concern: Security

- Urban trails do not increase crime
- Urban trails are regarded as neighborhood improvements
 - (**“Bike Path Phobia”, Parks and Recreation Magazine 2000**)
- Trails reduce crime by cleaning up the unmaintained landscape
- Attract those people who will use the trail for recreation
- Police report lack of crime by prohibiting vehicle use, due to no access of a criminal/escape mode
 - (**“Rail-Trails and Safe Communities”, Rails to Trails Conservancy, January 1998**)
- Litter not an issue on developed and maintained trails
- 97% had no major crime issues over a two-year period
 - (**National Survey of 372 trails in 1995-96**)
- When trail is proposed: 75% of adjacent homeowners had concerns
- After trail is established: <6% of adjacent homeowners had concerns
 - (**MN study of trail-adjacent landowner attitudes to vandalism and crime**)
- 1.25 break-ins per year, less than neighborhood average
 - (**Burke Gilman Trail, WA – 12 miles/8 years of data**)
- 65% of adjacent residents say that the trail enhanced quality of life for the neighborhood
 - (**Brushy Creek, Santa Rosa, CA**)

Concern: Property Values

Most highly visited, desirable, and expensive locations to live adjacent to trails and parks:



Beach Trails – Millions of visitors / homes above \$2 million



Central Park, New York – Most expensive/ desired homes in the Country



Augustus Hawkins Park, Compton - Property values drastically increased after construction



Griffith Park, Los Angeles



Presidio, San Francisco – High visitation, high property values



San Gabriel River Greenway, Lakewood – Residents became supportive after being fearful and have taken down their block walls

Concern: Property Values

- \$510 higher median home values for every $\frac{1}{4}$ mile closer to a trail in Minneapolis-St. Paul
- 11% higher values for homes within $\frac{1}{2}$ mi. of Monon Trail (IN)
- 6% higher values *near* trail; 0% difference *adjacent* to Burke-Gillman Trail (WA) – compared to similar homes not near
- 61% of owners adjacent to Luce Line Trail (MN) noted an increase in their property value as a result of the trail
- Realizing the selling power of greenways, developers added \$5,000 to the price of 40 homes adjacent to the regional trail and they were the first to sell.
- Developers also brand developments to include the trail as an amenity and part of its identity. (Iron Horse Trail, Concord CA)
- Lots adjacent to trail sold faster and for average of 9% more than properties not located next to the trail (WN).



Next Steps

- Determine mitigation techniques
- Confirm alignment of pedestrian bridge and trail
- Begin Construction Documents (CDs)
- Complete CEQA Amendment
- Follow-up Community Meeting / 30% CDs



Questions

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